

EXHIBIT B

DRAWINGS

IMPROVEMENT PLANS FOR
PORT OF STOCKTON - WEST COMPLEX
RAILROAD TRACK EXTENSION
ROUGH & READY ISLAND, STOCKTON, CALIFORNIA

GENERAL NOTES

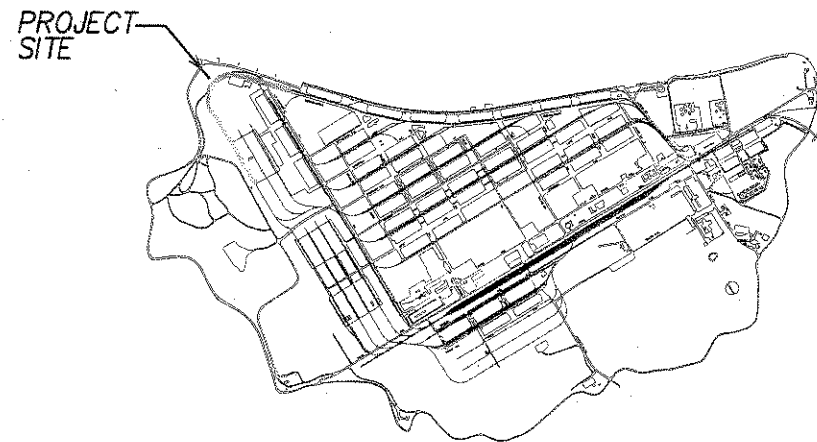
1. ALL WORK SHALL CONFORM TO THE CITY OF STOCKTON AND CALTRANS CONSTRUCTION SPECIFICATIONS, THE CITY OF STOCKTON STANDARD PLANS AND UNION PACIFIC RAILROAD SPECIFICATIONS.
2. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY FOR ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.
3. THE EXISTING UNDERGROUND UTILITIES SHOWN WERE PLOTTED FROM EXISTING AVAILABLE RECORDS OBTAINED FROM THE UTILITY COMPANIES. NEITHER THE OWNER NOR THE ENGINEER SHALL BE RESPONSIBLE FOR THE ACCURACY OF SUCH RECORDS AND DO NOT GUARANTEE THAT ALL UNDERGROUND FACILITIES ARE LOCATED PRECISELY AS SHOWN. PRIOR TO BEGINNING CONSTRUCTION, IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO ASCERTAIN FROM ALL UTILITY COMPANIES THE FIELD LOCATION OF THEIR UNDERGROUND FACILITIES. ALL KNOWN OWNERS OF UNDERGROUND FACILITIES IN THE AREA CONCERNED SHALL BE ADVISED OF PROPOSED WORK AT LEAST 72 HOURS PRIOR TO THE START OF ACTUAL EXCAVATION.
4. EXCAVATION SAFETY REQUIREMENTS INCLUDE SUBMITTING A CAL OSHA PERMIT OR LETTER OF NOTIFICATION TO THE CITY BEFORE EXCAVATION OF OVER FIVE FEET DEEP. REFER TO CITY CONSTRUCTION SPECIFICATIONS SEC. 8.19.04 "EXCAVATION SAFETY" FOR ADDITIONAL REQUIREMENTS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FROM DAMAGE ALL EXISTING IMPROVEMENTS THAT ARE TO REMAIN. SUCH IMPROVEMENTS THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT HIS EXPENSE.
6. PRIOR TO COMMENCING ANY WORK, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE EACH UTILITY COMPANY LOCATE IN THE FIELD THEIR MAIN AND SERVICE LINES. THE CONTRACTOR SHALL NOTIFY MEMBERS OF THE UNDERGROUND SERVICE ALERT (U.S.A.) 48 HOURS IN ADVANCE OF ANY EXCAVATION WORK BY CALLING 800-642-2444.
7. PRIOR TO SUBMITTING A BID, THE CONTRACTOR SHALL PERFORM THE NECESSARY EARTHWORK CALCULATIONS TO DETERMINE THE AMOUNT OF CUT AND FILL AND RESULTANT EXCESS OR SHORTAGE OF DIRT ONSITE, AND NOTIFY THE CIVIL ENGINEER OF HIS RESULTS. THIS MAY NOT BE A BALANCED SITE.
8. APPROPRIATE DUST CONTROL IS TO BE PROVIDED FOR THE ALLEVATION OR PREVENTION OF A DUST NUISANCE AT THE CONTRACTOR'S EXPENSE. THE PORT OF STOCKTON PROVIDES CONSTRUCTION WATER AT NO COST TO THE CONTRACTOR.
9. ALL CONSTRUCTION STAKING SHALL BE DONE BY DILLON & MURPHY. STAKING BY OTHERS SHALL BE AT THEIR OWN RISK AND DILLON & MURPHY IS NOT RESPONSIBLE FOR FIELD CHANGES.
10. CONTRACTOR SHALL GIVE THE CIVIL ENGINEER A MINIMUM OF 48 HOURS NOTIFICATION PRIOR TO ANY AND ALL STAKING.
11. ALL EXISTING FENCES ARE TO REMAIN, UNLESS NOTED. IF FENCES NEED TO BE REMOVED DURING PAVING, THEY ARE TO BE REPLACED IN KIND.
12. IT IS THE INTENT OF THE PORT OF STOCKTON TO HAVE THE SUCCESSFUL BIDDER PREPARE THEIR BID ANTICIPATING THAT THEY WILL BE PROVIDING A COMPLETE JOB AS SHOWN ON THE PLANS AND DESCRIBED IN THE SPECIFICATIONS.
13. THESE PLANS COMPLY WITH THE 2007 CALIFORNIA BUILDING CODE AND CALIFORNIA ELECTRICAL CODE.
14. UNLESS ALTERNATIVE MEASURES ARE PROVIDED FOR PROVIDING FIRE DEPARTMENT ACCESS (I.E., PORT POLICE ESCORT), ALL MANUALLY OPENED GATES SHALL BE PROVIDED WITH A BREAKAWAY TYPE LOCK OR A FIRE PADLOCK DEVICE.
15. DILLON & MURPHY, CONSULTING CIVIL ENGINEERS WILL NOT PROVIDE ELECTRONIC FILES OR VERTICAL AND HORIZONTAL CONTROL POINTS TO BE USED IN PREPARATION OF "GPS STAKING MODELS" OR FOR USE IN "GPS CONSTRUCTION STAKING" ACTIVITIES TO ANY INDIVIDUAL OR COMPANY WHO IS NOT LICENSED IN THE STATE OF CALIFORNIA TO PRACTICE CIVIL ENGINEERING AND/OR PROFESSIONAL LAND SURVEYING.
16. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INCLUDE ANY AND ALL REQUIREMENTS OF UNION PACIFIC RAILROAD AND AREMA CONSTRUCTION SPECIFICATIONS IN THEIR BID. ANY QUESTIONS REGARDING SPECIFICATIONS OR OMISSIONS ON THESE PLANS SHALL BE ADDRESSED AT THE PRE-BID CONFERENCE. THE CONTRACTOR'S BID WILL CONSIDER ALL U.P.R.R. AND AREMA REQUIREMENTS, WHETHER SHOWN ON THESE PLANS AND SPECIFICATIONS OR NOT.
17. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FULFILL ANY SWPPP AND BMP REQUIREMENTS.

INDEX OF SHEETS

<u>SHEET NUMBER</u>	<u>SHEET NAME</u>
	<u>CIVIL</u>
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4	RAILROAD SPUR-PLAN AND PROFILE STA. 00+00 TO STA. 15+89.22
5	RAILROAD SPUR-PLAN AND PROFILE STA. 15+89.22 TO STA. 32+97
6	GRADING DETAILS SHEET
7	EROSION CONTROL PLAN
8	TRAFFIC CIRCULATION PLAN
9-10	CROSS SECTIONS

RAILROAD GENERAL NOTES:

1. ALL RAIL MUST BE 115 LB OR LARGER WITH ONLY NEW OR NUMBER 1 RELAY, NO SHORTS OF LESS THAN 39 FEET EXCEPT FOR CLOSURE RAILS AND FOR RAIL JOINT STAGGER. RAIL MUST BE INSTALLED WITH BEST SIDE IN ON THE GAUGE SIDE. CURVES MAY BE LAID AT 56 3/4 INCHES GAUGE. CONTRACTOR REQUIRED CHECKING GAUGE ON ALL TRACK EVERY 60 FEET AND AT EACH JOINT IN A CURVE.
2. ALL TIE PLATE'S DOUBLE SHOULDER
3. COMPROMISE RAIL JOINTS MUST BE FORGED AND OF THE PROPER SIZE AND PROPER STAMPING TO INDICATE RAIL SIZE.
4. ALL WALKWAYS CONFORM TO CPUC GO #118 AND ALL CLEARANCES TO COMPLY WITH CPUC GO #26.
5. NO GATE MAY BE OVER 10'. ANY GATE OPENING OF 20' MUST BE COMPRISED OF TWO 10' GATES. ANY GATE 20 FEET OR MORE MUST OPERATE WITH POWER GATE OPENERS. GATE POSTS MUST BE 10 FEET FROM CENTER LINE OF THE TRACK AND STOP SIGNS MUST BE PLACED ON BOTH SIDES. PROPER LATCHES TO HOLD GATES OPEN MUST BE PROVIDED AND CHAIN OF 3/8 OR LARGER MUST BE WELDED TO GATE TO ALLOW FOR THE CCT LOCK TO BE APPLIED.
6. TIES: ALL TIES MUST BE INDUSTRIAL GRADE OR BETTER 7" X 8" X 8 1/2" OF MIXED HARDWOODS WITH END PLATING. SWITCH TIES MUST BE MIXED HARDWOOD OR HEMLOCK ONLY, END PLATED. NO RELAY TIES UNLESS APPROVED BY CCT GENERAL MANAGER. (FOR MARINE HIGHWAY TRACK, NO RELAY TIES.)
7. SWITCHES MUST HAVE 1 EXTRA GAUGE PLATE 2 TIES FROM POINT OF SWITCH.
8. RAIL ANCHORS: EVERY THIRD TIE ON JOINTED RAIL AND EVERY 2ND TIE ON WELDED RAIL. ALL TURNOUTS 100% RAIL ANCHORS.
9. THE ONLY SWITCH STANDS PERMISSIBLE ARE RACOR #22 STAND WITH INTEGRAL FOOT LATCH AND BACK SAVER HANDLES. TARGETS TO BE REFLECTORIZED RED AND WHITE.
10. SPIKING IN CURVES: ALL CURVES SHALL BE SPIKED WITH 4 SPIKES PER TIE. IN CURVES OF 6 DEGREES OR MORE, RATTLE OR HOLD DOWN SPIKE MUST BE USED ON THE OUTSIDE OF THE PLATE.
11. ROAD CROSSINGS: CONCRETE PANEL, HEAVY DUTY OMEGA, LAG DOWN WITH 10' HARDWOOD TIES WITH 10' TIES TO EXTEND FOR 5 TIES EITHER SIDE OF THE CROSSING. END OF PANELS TO MEET CPUC REQUIREMENTS FOR A 3:1 SLOPE WITH ASPHALT. ALL TIES MUST HAVE 4 SPIKES. USED CONCRETE PANELS MAY BE INSTALLED IF APPROVED BY THE CCT GENERAL MANAGER.
12. ALL ROAD MARKINGS AND SIGNAGE AS REQUIRED BY CPUC ORDER AND MUCTD MUST BE APPLIED AT ALL ROAD CROSSINGS.
13. ALL SWITCHES PER PLAN MUST HAVE ADJUSTABLE BRACES. SELF GUARDED FROGS ONLY ON APPROVAL OF THE CCT GENERAL MANAGER.
14. IF TRACK IS LAID WITH WELDED RAIL, CONTRACTOR TO PROVIDE REPORT OF RAIL DISTRESSING AND FOLLOW UPRR DISTRESSING GUIDELINES.
15. TRACK BED TO BE MINIMUM OF 24 FEET, AND ALL VEGETATION AND TREES CUT BACK TO A MINIMUM OF 30 FEET FROM CENTER OF TRACKS.
16. ALL TRACK TO HAVE FILTER FABRIC INSTALLED BETWEEN SUB-BALLAST AND BALLAST SECTION. USE 10 OZ OR 12 OZ 15 FEET WIDE NON-WOVEN FABRIC 130 TO 145 MILS THICK.
17. NEW BOLTS ARE REQUIRED AT ALL JOINTS.
18. (2) GAUGE PLATES ARE REQUIRED PER UPRR STANDARDS AT ALL SWITCHES.
19. ALL BALLAST TO BE A MINIMUM OF AREMA SIZE 5 AND SIZE 2 AT SWITCH AND WALKWAYS PER P.U.C. GENERAL ORDER #118.



VICINITY MAP
NOT TO SCALE

OWNER:

PORT OF STOCKTON
 2201 W. WASHINGTON STREET
 STOCKTON, CA 95203
 (209) 946-0246

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 CONSULTING CIVIL ENGINEERS
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 847 N. CLIFF AVENUE, SUITE A-2, LODI, CALIFORNIA, 95240 (209) 334-6613

PORT OF STOCKTON-WEST COMPLEX
RAILROAD TRACK EXTENSION

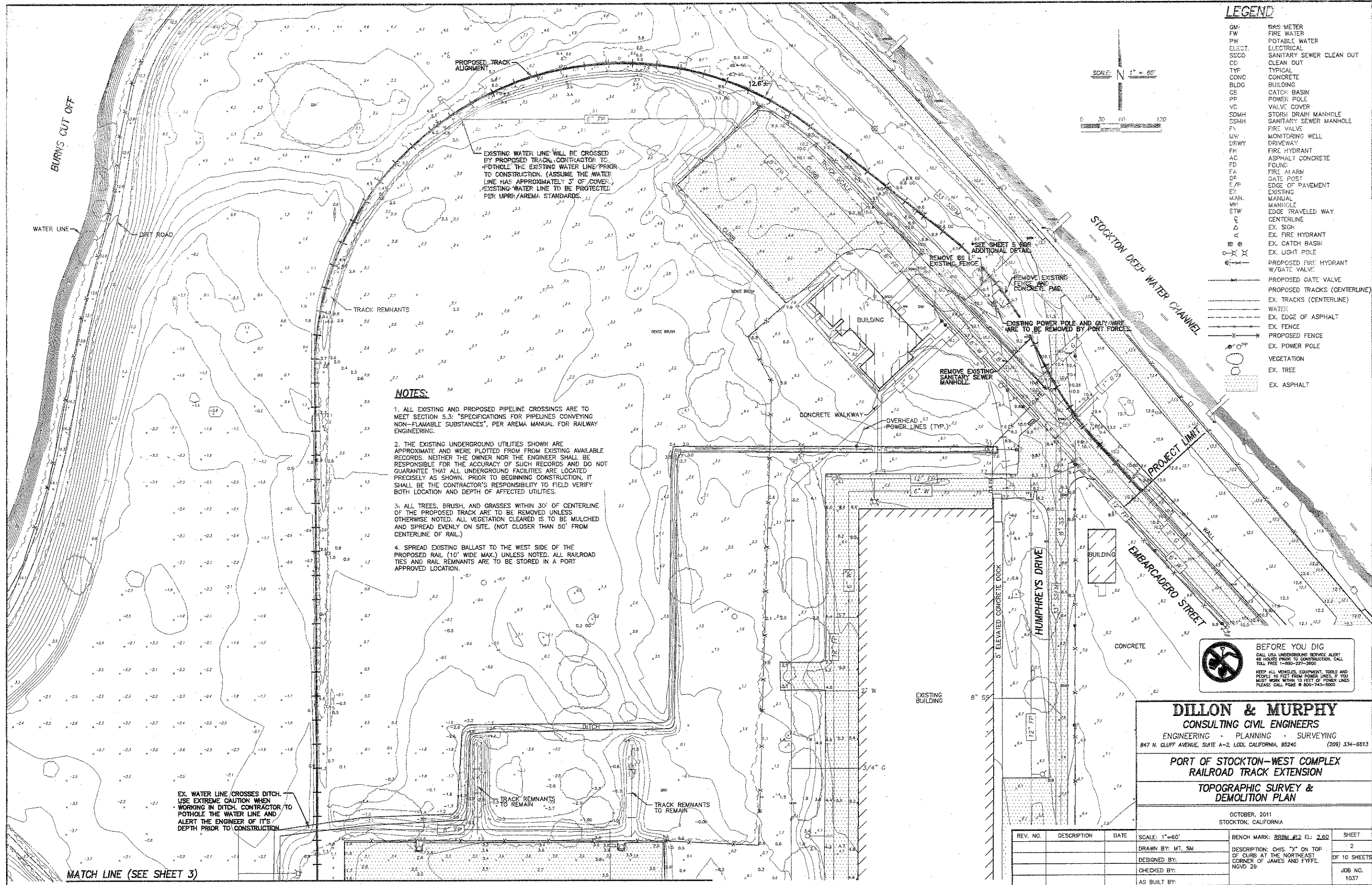
TITLE SHEET

OCTOBER, 2011
 STOCKTON, CALIFORNIA

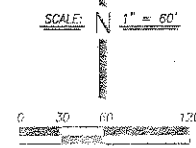
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			DRAWN BY: M.T. LM	DESCRIPTION: CHS. "X" ON TOP OF CURB AT THE NORTHEAST CORNER OF JAMES AND FYPPE, NGVD 29.	1
			DESIGNED BY:		OF 10 SHEETS
			CHECKED BY: JM		JOB NO.
			AS BUILT BY:		1037

CONTRACT NO. 12-11-1





- LEGEND**
- GM- GAS METER
 - FW- FIRE WATER
 - PW- POTABLE WATER
 - ELECT- ELECTRICAL
 - SSCO- SANITARY SEWER CLEAN OUT
 - CO- CLEAN OUT
 - TYP- TYPICAL
 - CONC- CONCRETE
 - BLDG- BUILDING
 - CB- CATCH BASIN
 - PP- POWER POLE
 - VC- VALVE COVER
 - SDMH- STORM DRAIN MANHOLE
 - SSMH- SANITARY SEWER MANHOLE
 - FV- FIRE VALVE
 - MW- MONITORING WELL
 - DRWY- DRIVEWAY
 - PH- FIRE HYDRANT
 - AC- ASPHALT CONCRETE
 - FOUND- FOUND
 - FA- FIRE ALARM
 - GP- GATE POST
 - E/P- EDGE OF PAVEMENT
 - EX- EXISTING
 - MAN- MANUAL MANHOLE
 - ETW- EDGE TRAVELED WAY
 - C- CENTERLINE
 - EX- EX. FIRE HYDRANT
 - EX- EX. CATCH BASIN
 - EX- EX. LIGHT POLE
 - PH- PROPOSED FIRE HYDRANT
 - W/GATE VALVE- W/GATE VALVE
 - PROPOSED GATE VALVE- PROPOSED GATE VALVE
 - PROPOSED TRACKS (CENTERLINE)- PROPOSED TRACKS (CENTERLINE)
 - EX. TRACKS (CENTERLINE)- EX. TRACKS (CENTERLINE)
 - WATER- WATER
 - EX. EDGE OF ASPHALT- EX. EDGE OF ASPHALT
 - EX. FENCE- EX. FENCE
 - PROPOSED FENCE- PROPOSED FENCE
 - EX. POWER POLE- EX. POWER POLE
 - VEGETATION- VEGETATION
 - EX. TREE- EX. TREE
 - EX. ASPHALT- EX. ASPHALT



EXISTING WATER LINE WILL BE CROSSED BY PROPOSED TRACK. CONTRACTOR TO POT-HOLE THE EXISTING WATER LINE PRIOR TO CONSTRUCTION. (ASSUME THE WATER LINE HAS APPROXIMATELY 3' OF COVER). EXISTING WATER LINE TO BE PROTECTED PER UPRR/AREMA STANDARDS.

- NOTES:**
1. ALL EXISTING AND PROPOSED PIPELINE CROSSINGS ARE TO MEET SECTION 5.3: "SPECIFICATIONS FOR PIPELINES CONVEYING NON-FLAMMABLE SUBSTANCES", PER AREMA MANUAL FOR RAILWAY ENGINEERING.
 2. THE EXISTING UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE AND WERE PLOTTED FROM EXISTING AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER SHALL BE RESPONSIBLE FOR THE ACCURACY OF SUCH RECORDS AND DO NOT GUARANTEE THAT ALL UNDERGROUND FACILITIES ARE LOCATED PRECISELY AS SHOWN. PRIOR TO BEGINNING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY BOTH LOCATION AND DEPTH OF AFFECTED UTILITIES.
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 4. SPREAD EXISTING BALLAST TO THE WEST SIDE OF THE PROPOSED RAIL (10' WIDE MAX.) UNLESS NOTED. ALL RAILROAD TIES AND RAIL REMNANTS ARE TO BE STORED IN A PORT APPROVED LOCATION.

EX. WATER LINE / CROSSES DITCH. USE EXTREME CAUTION WHEN WORKING IN DITCH. CONTRACTOR TO POT-HOLE THE WATER LINE AND ALERT THE ENGINEER OF ITS DEPTH PRIOR TO CONSTRUCTION.

BEFORE YOU DIG
 CALL USA UNDERGROUND SERVICE ALERT
 48 HOURS PRIOR TO CONSTRUCTION. CALL
 TOLL FREE 1-800-227-2600.
 KEEP ALL WEAPONS, EQUIPMENT, TOOLS AND
 PEOPLE 10 FEET FROM POWER LINES. IF YOU
 MUST WORK WITHIN TO FEET OF POWER LINES
 PLEASE CALL PSE&E 800-743-6900

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**PORT OF STOCKTON-WEST COMPLEX
 RAILROAD TRACK EXTENSION
 TOPOGRAPHIC SURVEY &
 DEMOLITION PLAN**

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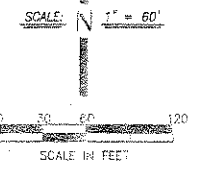
LEGEND

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- L/P EDGE OF PAVEMENT
- EX EXISTING
- MAN. MANHOLE
- NH
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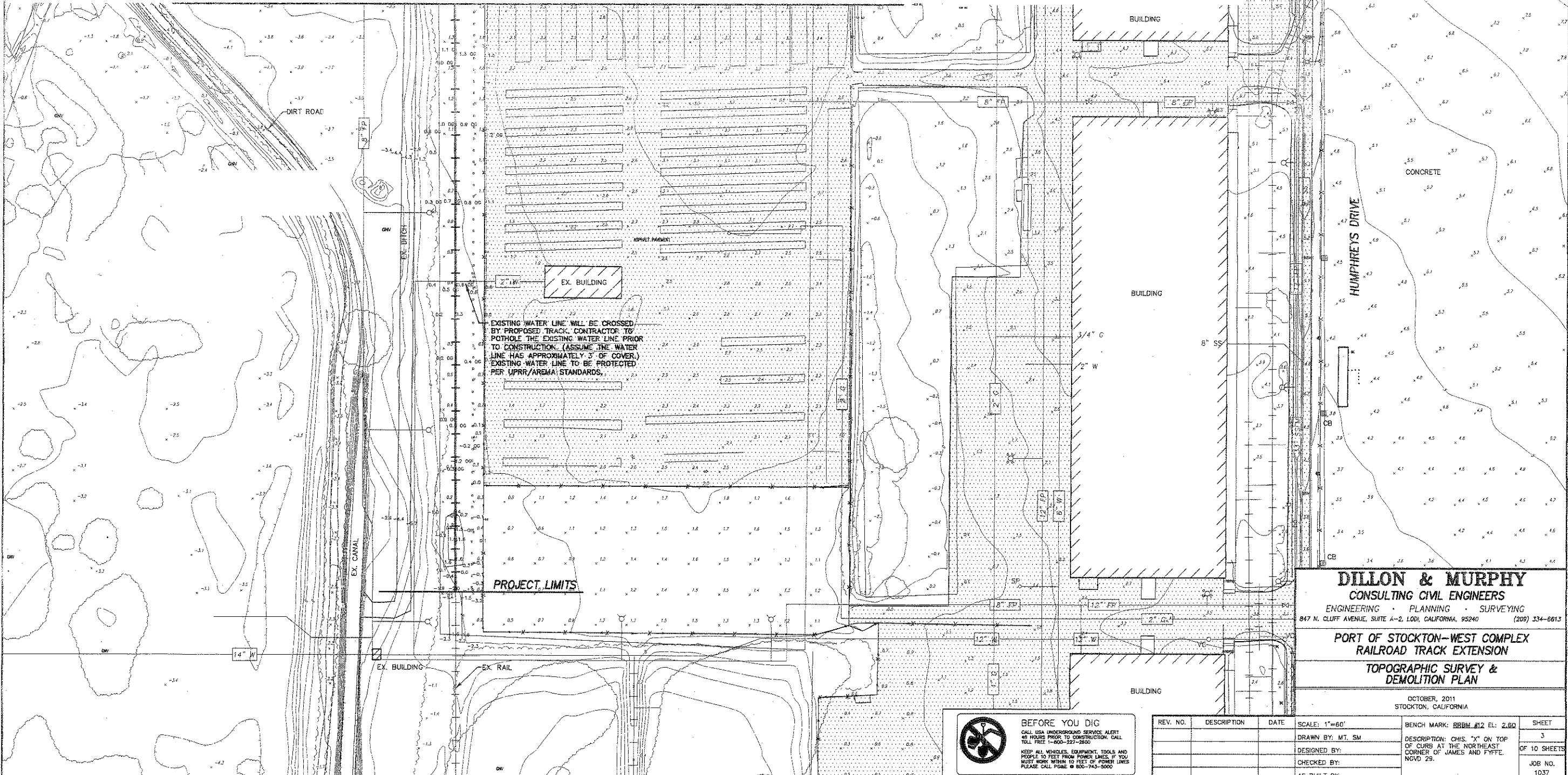
- CENTERLINE
- EX. SIGN
- EX. FIRE HYDRANT
- EX. CATCH BASIN
- EX. LIGHT POLE
- PROPOSED FIRE HYDRANT W/GATE VALVE
- PROPOSED GATE VALVE
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NOTES

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MATCH LINE (SEE SHEET 2)



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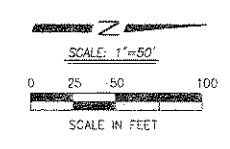
PORT OF STOCKTON-WEST COMPLEX
RAILROAD TRACK EXTENSION
TOPOGRAPHIC SURVEY &
DEMOLITION PLAN

OCTOBER, 2011
 STOCKTON, CALIFORNIA

BEFORE YOU DIG
 CALL USA UNDERGROUND SERVICE ALERT
 48 HOURS PRIOR TO CONSTRUCTION. CALL
 TOLL FREE 1-800-227-2860
 KEEP ALL VEHICLES, EQUIPMENT, TOOLS AND
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 PLEASE CALL PG&E @ 800-743-5000

REV. NO.	DESCRIPTION	DATE	SCALE: 1"=60'	BENCH MARK: BBBM #12 EL: 2.60	SHEET
					3
					OF 10 SHEETS
					JOB NO.
					1037

BEGIN IMPROVEMENTS
STA. 0+00

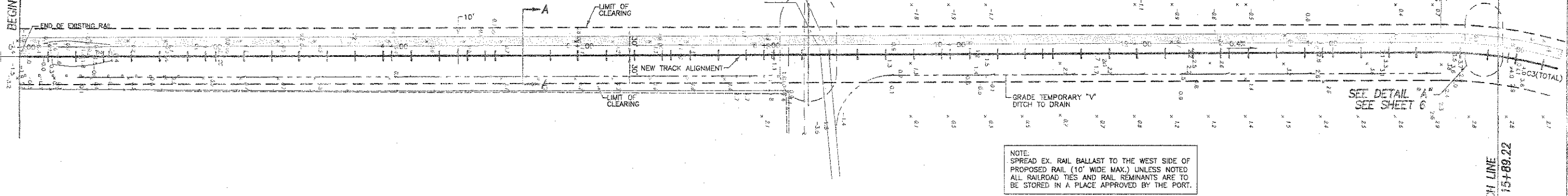


CURVE TABLE					
CURVE	RAI (DEG)	Δ	DEGREE	TAN	LENGTH
C1	455.2E	15°33'57"	12°30'00"	62.77'	124.77'
C2	455.2E	24°46'26"	12°30'00"	100.67'	168.58'
C3	455.2E	126°37'09"	12°30'00"	954.72'	1031.00'

SEE DETAIL "B"
SEE SHEET 6

SEE DETAIL "A"
SEE SHEET 6

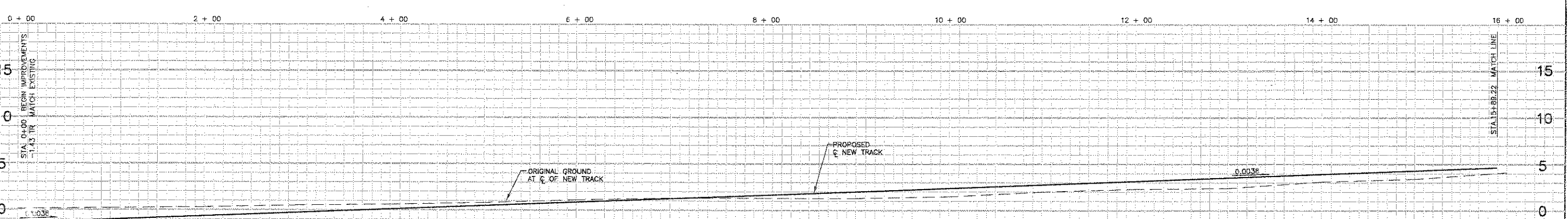
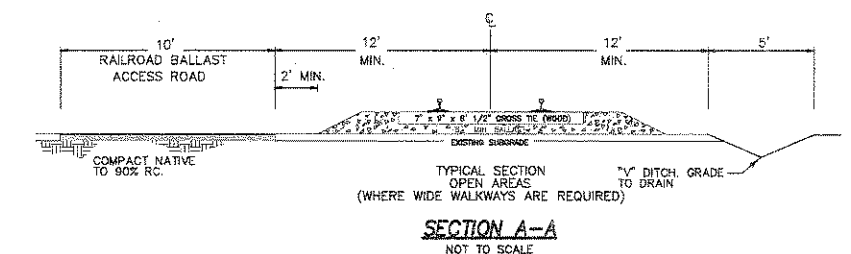
SEE SHEET 5



NOTE:
SPREAD EX. RAIL BALLAST TO THE WEST SIDE OF PROPOSED RAIL (10' WIDE MAX.) UNLESS NOTED ALL RAILROAD TIES AND RAIL REMAINANTS ARE TO BE STORED IN A PLACE APPROVED BY THE PORT.

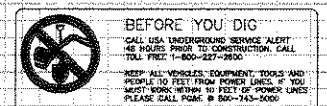
LEGEND

- | | | | |
|--------|--------------------------------|---------|------------------------------------|
| GM | GAS METER | EX MAN. | EXISTING MANUAL MANHOLE |
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| PP | POWER POLE | ⊙ | EX. TRACKS (CENTERLINE) |
| VC | VALVE COVER | ⊙ | WATER |
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| SSMH | SANITARY SEWER MANHOLE | ⊙ | EX. FENCE |
| FV | FIRE VALVE | ⊙ | PROPOSED FENCE |
| MW | MONITORING WELL | ⊙ | EX. POWER POLE |
| DRWY | DRIVEWAY | ⊙ | VEGETATION |
| FH | FIRE HYDRANT | ⊙ | EX. TREE |
| AC | ASPHALT CONCRETE | ⊙ | PROPOSED GRAVEL ACCESS ROAD |
| FD | FOUND | | |
| FA | FIRE ALARM | | |
| GP | GATE POST | | |
| E/P | EDGE OF PAVEMENT | | |
| PC | POINT AT CURVE | | |
| PT | POINT AT TANGENT | | |
| BVC | BEGIN VERTICAL CURVE | | |
| EVC | END VERTICAL CURVE | | |
| PVI | POINT AT VERTICAL INTERSECTION | | |



EXISTING WATER LINE
DEPTH UNKNOWN

INSTALL 92 LF ~ 24" CLASS IV RCP CULVERT



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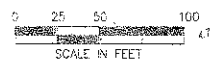
**PORT OF STOCKTON-WEST COMPLEX
RAILROAD TRACK EXTENSION**

**RAILROAD SPUR - PLAN AND PROFILE
STA. 0 + 00 TO STA. 15 + 89.22**

OCTOBER, 2011
STOCKTON, CALIFORNIA

REV. NO.	DESCRIPTION	DATE	SCALE: AS SHOWN	BENCH MARK: RBBM #12 EL. 2.80	SHEET
	DRAWN BY: MT			DESCRIPTION: CHIS. "X" ON TOP OF CURB AT THE NORTHEAST CORNER OF JAMES AND FYFFE, NGVD 29.	4
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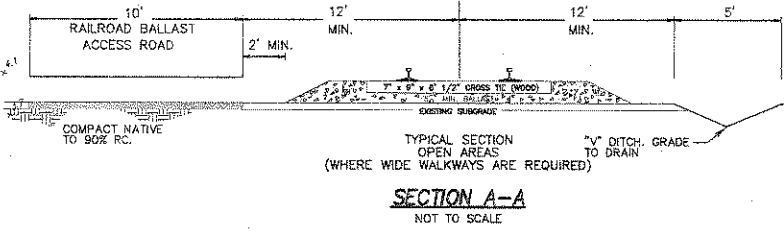
SCALE: 1" = 50'



LEGEND

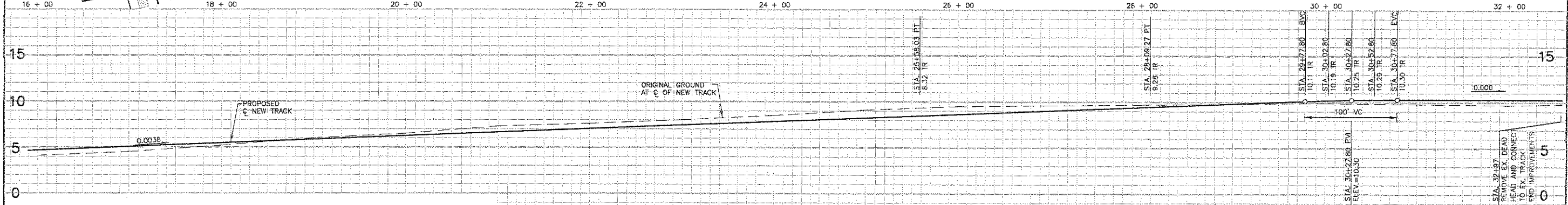
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FW	POTABLE WATER
ELECT.	ELECTRICAL
SSCO	SANITARY SEWER CLEAN OUT
CO	CLEAN OUT
TY	TYPICAL
CONC	CONCRETE
BLDC	BUILDING
CS	CATCH BASIN
PP	POWER POLE
VC	VALVE COVER
SDMH	STORM DRAIN MANHOLE
SSMH	SANITARY SEWER MANHOLE
FV	FIRE VALVE
MW	MONITORING WELL
DRWAY	DRIVEWAY
FV	FIRE HYDRANT
AC	ASPHALT CONCRETE
FC	FLOORING
FA	FIRE ALARM
GR	GUTTER
SP	SLICE OF PAVEMENT
PT	POINT AT CURVE
PT	POINT AT TANGENT
BVM	BEGIN VERTICAL CURVE
EVC	END VERTICAL CURVE
PIV	POINT AT VERTICAL INTERSECTION
EX	EXISTING
MAN	MANHOLE
MS	MANHOLE
ETW	EDGE TRAVELED WAY
CL	CENTERLINE
EX	EX. SIGN
EX	EX. FIRE HYDRANT
EX	EX. CATCH BASIN
EX	EX. LIGHT POLE
PROPOSED	PROPOSED FIRE HYDRANT W/GATE VALVE
PROPOSED	PROPOSED GATE VALVE
PROPOSED	PROPOSED TRACKS (CENTERLINE)
EX	EX. TRACKS (CENTERLINE)
WATER	WATER
EX	EX. EDGE OF ASPHALT
EX	EX. FENCE
PROPOSED	PROPOSED FENCE
EX	EX. POWER POLE
VEGETATION	VEGETATION
EX	EX. TREE
PROPOSED	PROPOSED GRAVEL ACCESS ROAD
PROPOSED	PROPOSED GRAVEL ACCESS ROAD

NOTE:
SPREAD EX. RAIL BALLAST TO THE WEST SIDE OF PROPOSED RAIL (10' WIDE MAX.) UNLESS NOTED ALL RAILROAD TIES AND RAIL REMNANTS ARE TO BE STORED IN A PLACE APPROVED BY THE PORT.



CURVE TABLE

CURVE	RADIUS	Δ	DEGREE	TAN	LENGTH
C1	458.26'	15°33'57"	12°30'00"	62.77'	124.77'
C2	458.26'	24°46'28"	12°30'00"	100.87'	198.59'
C3	458.26'	128°37'09"	12°30'00"	954.72'	1031.00'



CONSTRUCTION NOTES

1. ALL RAIL (INCLUDING SWITCHES) SHALL BE 115# OR HEAVIER.
2. ALL BALLAST TO BE A MINIMUM OF AREMA SIZE 5 AND SIZE 2 AT SWITCH AND WALKWAYS PER P.U.C. GENERAL ORDER #118.
3. ALL WALKWAYS TO COMPLY WITH P.U.C. GENERAL ORDER #118.
4. ALL CLEARANCES TO COMPLY WITH CPUC GO #26.
5. ALL ROAD CROSSINGS TO COMPLY WITH U.P.R.R. SPECIFICATIONS.
6. ALL EXISTING AND PROPOSED PIPELINE CROSSINGS ARE TO MEET SECTION 5.3: "SPECIFICATIONS FOR PIPELINES CONVEYING NON-FLAMABLE SUBSTANCES", PER AREMA MANUAL FOR RAILWAY ENGINEERING.
7. INSTALL ALL RAILROAD CROSSING SIGNAGE TO COMPLY WITH U.P.R.R. STANDARDS.
8. REMOVE EXISTING SANITARY SEWER MANHOLE. REMOVE EXISTING SANITARY SEWER 10 FEET EACH DIRECTION OF MANHOLE. BACKFILL AND COMPACT TO UPRR STANDARDS.
9. INSTALL ALL RAILROAD STRIPING TO COMPLY WITH U.P.R.R. SPECIFICATIONS.

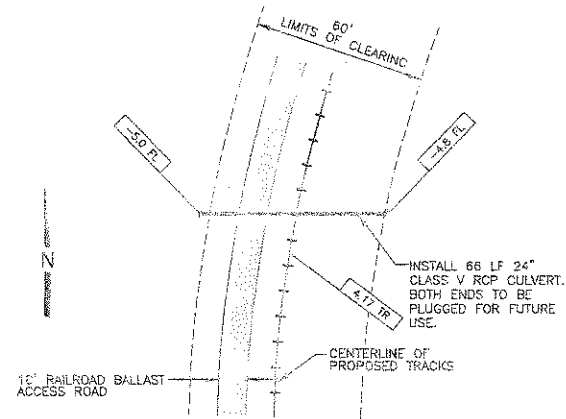


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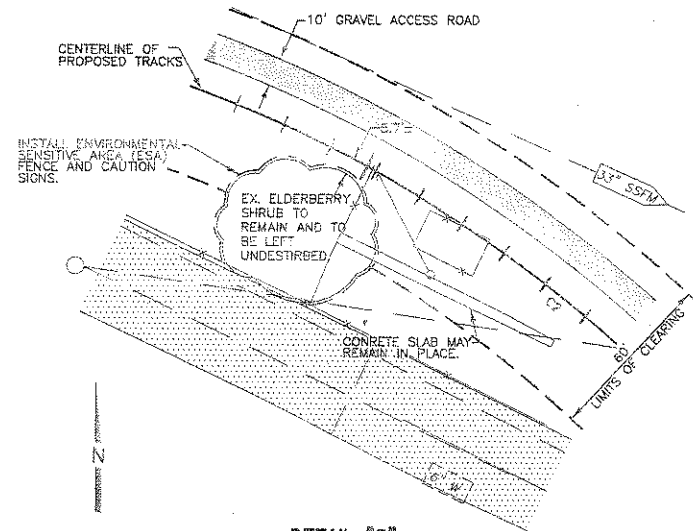
PORT OF STOCKTON-WEST COMPLEX RAILROAD TRACK EXTENSION
RAILROAD SPUR - PLAN AND PROFILE
STA. 15 + 89.22 TO STA. 32 + 97

OCTOBER, 2011
STOCKTON, CALIFORNIA

REV. NO.	DESCRIPTION	DATE	SCALE: 1"=50'	BENCH MARK: RRRM #12 EL.: 2.60	SHEET
	DRAWN BY: MT				5
	DESIGNED BY: MT				OF 10 SHEETS
	CHECKED BY: JM				JOB NO.
	AS BUILT BY:				1037

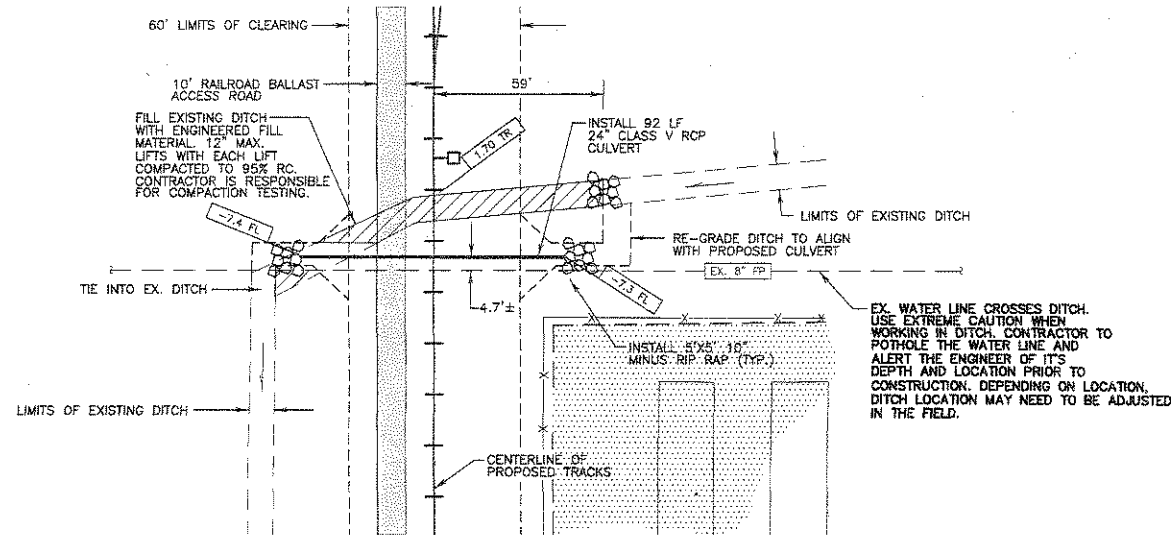


DETAIL "A"
SCALE: 1"=30'



DETAIL "C"
SCALE: 1"=30'

NOTE:
ALL EXISTING AND PROPOSED PIPELINE CROSSINGS ARE TO MEET SECTION 5.3: "SPECIFICATIONS FOR PIPELINES CONVEYING NON-FLAMMABLE SUBSTANCES", PER AREMA MANUAL FOR RAILWAY ENGINEERING.



DETAIL "B"
SCALE: 1"=30'

LEGEND

- | | | | |
|--------|--------------------------|---|------------------------------------|
| GM | GAS METER | ⊕ | CENTERLINE |
| FW | FIRE WATER | △ | EX. SIGN |
| PW | POTABLE WATER | ⊙ | EX. FIRE HYDRANT |
| ELECT. | ELECTRICAL | ⊗ | EX. CATCH BASIN |
| SSCO | SANITARY SEWER CLEAN OUT | ⊗ | EX. LIGHT POLE |
| CO | CLEAN OUT | ⊗ | PROPOSED FIRE HYDRANT W/GATE VALVE |
| TYP | TYPICAL | ⊗ | PROPOSED GATE VALVE |
| CONC | CONCRETE | ⊗ | PROPOSED TRACKS (CENTERLINE) |
| BLDG | BUILDING | ⊗ | EX. TRACKS (CENTERLINE) |
| CB | CATCH BASIN | ⊗ | WATER |
| PP | POWER POLE | ⊗ | EX. EDGE OF ASPHALT |
| VC | VALVE COVER | ⊗ | EX. FENCE |
| SDMH | STORM DRAIN MANHOLE | ⊗ | PROPOSED FENCE |
| SSMH | SANITARY SEWER MANHOLE | ⊗ | EX. POWER POLE |
| FV | FIRE VALVE | ⊗ | VEGETATION |
| MW | MONITORING WELL | ⊗ | EX. TREE |
| DRWY | DRIVEWAY | ⊗ | |
| FH | FIRE HYDRANT | ⊗ | |
| AC | ASPHALT CONCRETE | ⊗ | |
| FD | FOUND | ⊗ | |
| FA | FIRE ALARM | ⊗ | |
| GP | GATE POST | ⊗ | |
| E/P | EDGE OF PAVEMENT | ⊗ | |
| EX | EXISTING | ⊗ | |
| MAN. | MANHOLE | ⊗ | |
| ETW | EDGE TRAVELED WAY | ⊗ | |

NOTE: TO CONVERT NVGD 29 DATUM TO NAVD 88, ADD 2.04386 TO GRADES.

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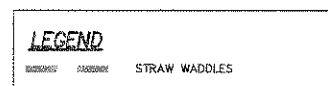
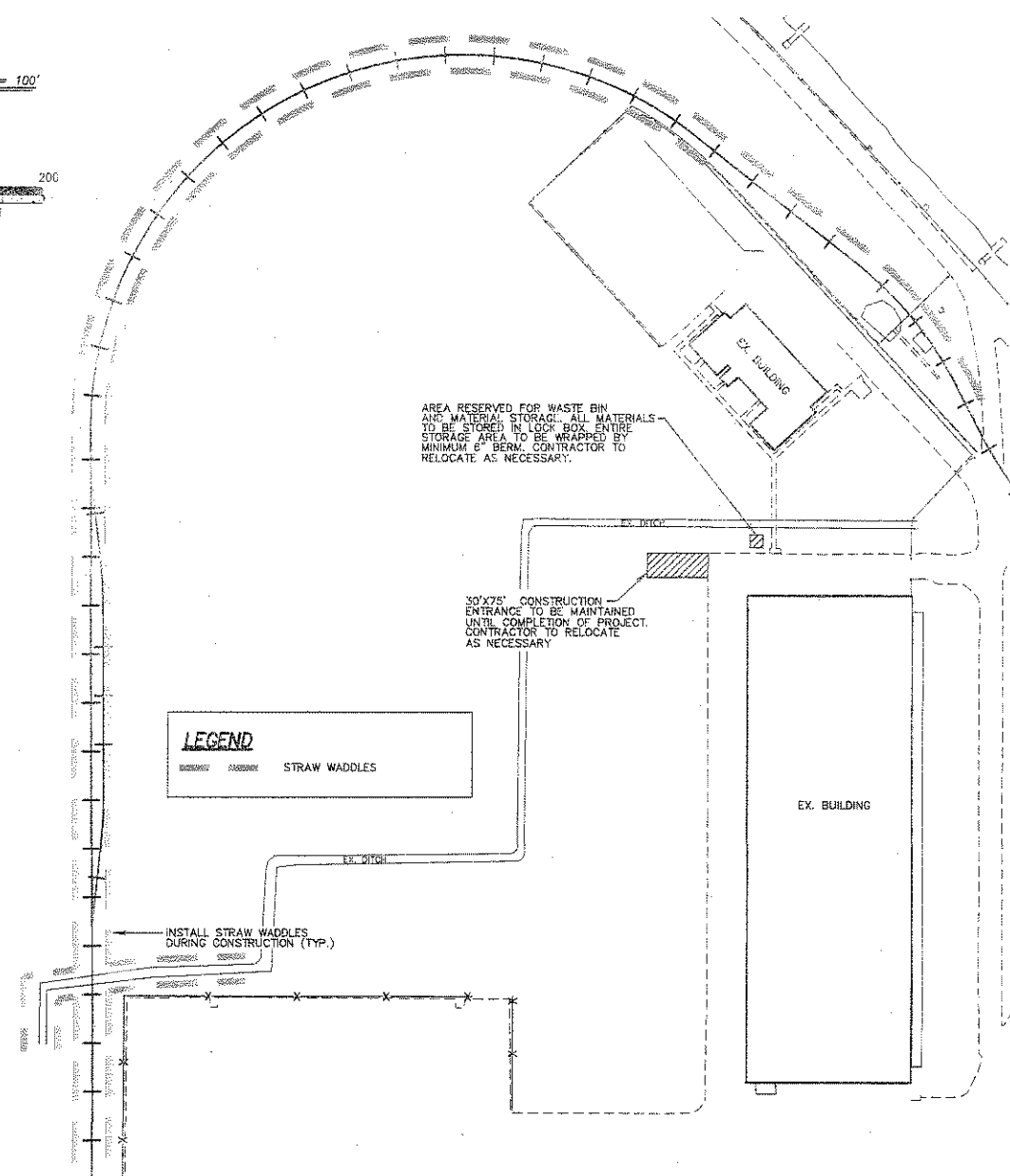
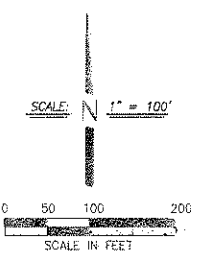
PORT OF STOCKTON-WEST COMPLEX RAILROAD TRACK EXTENSION

GRADING DETAILS SHEET

OCTOBER, 2011
STOCKTON, CALIFORNIA



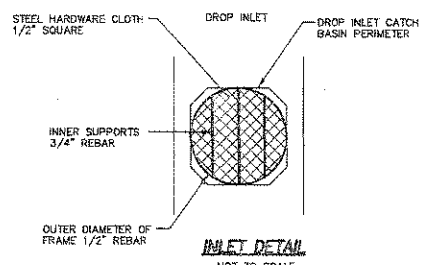
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	DRAWN BY: LM			DESCRIPTION: CHISEL "X" ON TOP OF CURB AT THE NORTHEAST CORNER OF JAMES AND FYFFE, NVGD 29.	6
	DESIGNED BY: LM				OF 10 SHEETS
	CHECKED BY: JM				JOB NO. 1037
	AS BUILT BY:				



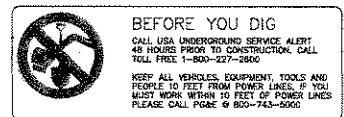
AREA RESERVED FOR WASTE BIN AND MATERIAL STORAGE. ALL MATERIALS TO BE STORED IN LOCK BOX. ENTIRE STORAGE AREA TO BE WRAPPED BY MINIMUM 6" BERM. CONTRACTOR TO RELOCATE AS NECESSARY.

30'x75' CONSTRUCTION ENTRANCE TO BE MAINTAINED UNTIL COMPLETION OF PROJECT. CONTRACTOR TO RELOCATE AS NECESSARY.

CONTRACTOR TO MAINTAIN 20' BUFFER AROUND ELDERBERRY SHRUBS WHERE POSSIBLE. INSTALL ENVIRONMENTAL SENSITIVE AREA (ESA) FENCE AND CAUTION SIGNS.



- NOTES:
1. DEVELOPER IS RESPONSIBLE FOR REPLACEMENT OF MISSING AND DAMAGED FILTER SCREENS UNTIL THE PROJECT IS ACCEPTED. A ROUTINE MAINTENANCE SCHEDULE IS TO BE MAINTAINED. FILTER SCREENS SHALL BE SECURED TO THE CATCH-BASIN DURING THIS PERIOD.
 2. COMPRESSED BETWEEN REBAR FRAME AND DIBC IS 1" THICK FILTER PAD MATERIAL. FILTER PAD MATERIAL IS DENSE TO RETAIN SILT YET POROUS ENOUGH TO ALLOW WATER DRAINAGE. THE FILTER SYSTEM IS WIRE TIED TO THE DIBC IN 2 OR MORE PLACES TO PREVENT SLIPPAGE OR REMOVAL OF UNIT FROM DIBC.
 3. THE FILTER PAD AND GRATE SHALL BE SECURELY ATTACHED TO THE DROP INLET BY WIRE OR TIE-WRAPS.
 4. INLET FILTERS SHALL BE INSPECTED WEEKLY AND AFTER EACH RAINFALL. REPAIRS AND SEDIMENT AND DEBRIS REMOVAL SHALL BE MADE AS NECESSARY.
 5. INLET FILTERS SHALL BE "CONSTRUCTION" STORM DRAIN FILTERS BY POLLUTION SOLUTION.

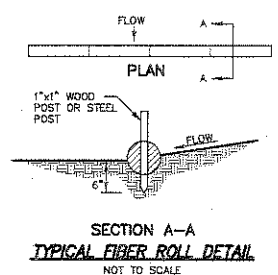


DUST CONTROL NOTES:

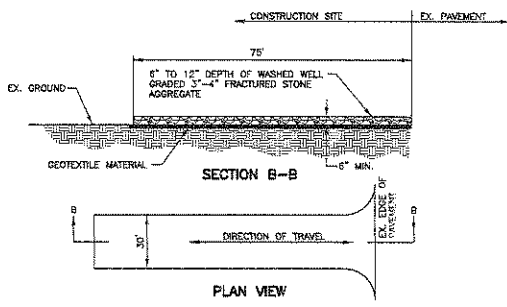
1. DUST CONTROL IS THE CONTRACTOR'S SOLE RESPONSIBILITY WHEN WORKING ON-SITE. THE WORK SHOWN ON THIS SHEET SHALL BE INCLUDED IN THE BID. THE CONTRACTOR SHALL ALSO WATER THE SITE AS AN EXTRA, WHEN REQUESTED BY THE PORT OF STOCKTON.
2. ALL MOTORS SHALL BE KEPT PROPERLY TUNED AND MAINTAINED.
3. EARTH MOVING ACTIVITIES SHALL CEASE IF WIND EXCEEDS 20 MPH AVERAGE OVER ONE HOUR.
4. ON-SITE VEHICLE SPEEDS SHALL BE LIMITED TO 15 MPH.
5. ALL OPERATIONS SHALL COMPLY WITH THE REQUIREMENTS OF THE SAN JOAQUIN COUNTY UNIFIED AIR POLLUTION CONTROL DISTRICT.
6. ALL AREAS WITH VEHICLE TRAFFIC SHOULD BE WATERED PERIODICALLY FOR STABILIZATION OF DUST EMISSIONS.
7. STREETS ADJACENT TO THE PROJECT SITE SHOULD BE SWEEP AS NEEDED TO REMOVE SILT WHICH MAY HAVE ACCUMULATED FROM CONSTRUCTION ACTIVITIES.
8. THE AREA DISTURBED BY CLEARING, EARTH MOVING OR EXCAVATION ACTIVITIES SHOULD BE MINIMIZED AT ALL TIMES.
9. ALL MATERIAL TRANSPORTED ON OR OFF-SITE SHALL EITHER BE SUFFICIENTLY WATERED OR SECURELY COVERED TO PREVENT EXCESSIVE AMOUNTS OF DUST.
10. WHEN CONDITIONS ARE DRY, ALL MATERIAL EXCAVATED OR GRADED SHOULD BE SUFFICIENTLY WATERED TO PREVENT EXCESSIVE AMOUNTS OF DUST. WATERING SHOULD OCCUR AT LEAST TWICE A DAY WITH COMPLETE COVERAGE, PREFERABLY IN THE LATE MORNING AND AFTER WORK IS COMPLETED FOR THE DAY.

STORM WATER POLLUTION PREVENTION PLAN

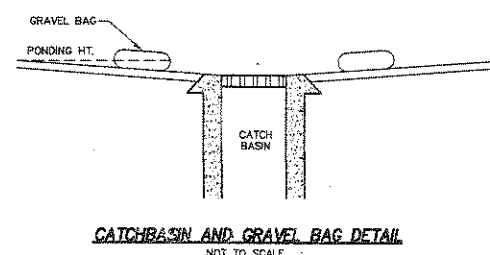
- WITH THE EXCEPTION OF PROPERLY DESILTED WATER FROM SITE DEWATERING, THE CONTRACTOR SHALL NOT DISCHARGE ANY MATERIALS AND/OR LIQUIDS TO THE STORM DRAINAGE SYSTEM. ACTIVITIES OF PARTICULAR CONCERN ARE:
- A. CONTRACTOR SHALL PROVIDE A CONCRETE TRUCK CLEAN-OUT AREA. PORT OF STOCKTON INSPECTOR APPROVED BEST MANAGEMENT PRACTICES SHALL AT ALL TIMES BE FULLY IMPLEMENTED AT AND AROUND THE CLEAN-OUT AREA.
 - B. TACK COAT AND PRIME COAT ASPHALTS SHALL BE CAREFULLY SPRAYED AND ANY EXCESS MATERIAL SPILLED SHALL BE CLEANED UP IMMEDIATELY BY PROPER METHODS.
 - C. ALL EQUIPMENT REFUELING IN THE PROJECT AREA SHALL BE CAREFULLY DONE TO AVOID SPILLAGE. ANY SPILLS SHALL BE CONTAINED AND CLEANED UP IMMEDIATELY IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS. ALL FUELING VEHICLES SHALL BE EQUIPPED WITH SPILL CLEANUP MATERIALS AND EQUIPMENT.
 - D. CONTRACTOR SHALL CONTROL AND PROPERLY DISPOSE OF ALL LIQUIDS DURING SAWCUTTING ACTIVITIES.



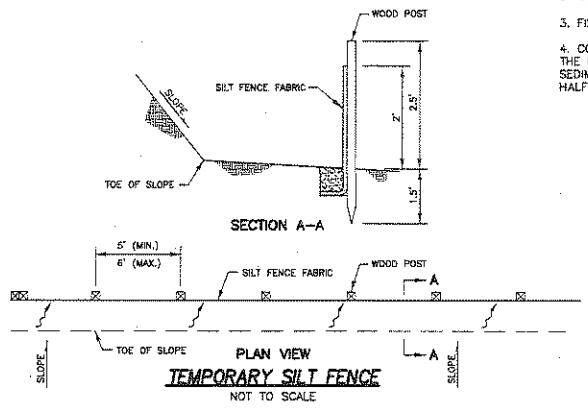
- NOTES:
1. FILTER ROLLS SHALL BE CONSTRUCTED LONG ENOUGH TO EXTEND ACROSS THE EXPECTED FLOW PATCH.
 2. SUPPORT POSTS SHALL BE A MINIMUM 24" LONG 1"x1" WOOD POSTS DRIVEN A MINIMUM OF 6" INTO THE GROUND. POSTS SHALL BE SPACED A MAXIMUM OF 4 FEET APART.
 3. FILTER ROLL SHALL BE IMBEDDED A MINIMUM OF 3" INTO EXISTING GROUND.
 4. CONTRACTOR SHALL MAKE INSPECTIONS WEEKLY DURING THE WET SEASON, MONTHLY DURING THE DRY SEASON, AND IMMEDIATELY AFTER EACH RAINFALL TO DETERMINE IF REPAIRS AND SEDIMENT REMOVAL IS REQUIRED. SEDIMENT SHALL BE REMOVED BEFORE IT HAS REACHED ONE HALF THE HEIGHT OF THE FILTER ROLL.



- NOTES:
1. STABILIZED CONSTRUCTION SITE ACCESS SHALL BE CONSTRUCTED OF 3" TO 4" WASHED, WELL-GRADED FRACTURED STONE AGGREGATE. MATERIAL SHALL BE PLACED TO A MINIMUM THICKNESS OF 6", SLOPED AWAY FROM THE ROADWAY.
 2. LENGTH OF ENTRANCE VARIES DUE TO SITE ENTRANCE RESTRAINTS. WIDTH SHALL VARY AS NECESSARY TO COVER ALL VEHICULAR INGRESS AND EGRESS. PROVIDE AMPLE TURNING RADI.
 3. THE ENTRANCE SHALL BE KEPT IN GOOD CONDITION BY OCCASIONAL TOP DRESSING WITH MATERIAL AS SPECIFIED IN NOTE 1.
 4. ACCESSES SHALL BE INSPECTED WEEKLY, AND AFTER EACH RAINFALL, AND MAINTAINED AS NECESSARY. PERIODIC TOP DRESSING SHALL BE DONE AS NEEDED.
 5. ANY DEBRIS, SEDIMENT OR ROCK TRACKED ONTO PUBLIC STREETS WILL BE SWEEPED UP BY THE END OF EACH WORKDAY.



- NOTES:
1. GRAVEL BAGS SHALL BE WOVEN GEOTEXTILE FABRIC. (12"x18"± AND 15 LBS± OF 1" MAX. ROCK)
 2. CONSTRUCT ON GENTLY SLOPING STREET WHERE WATER CAN POND AND ALLOW SEDIMENT TO SEPARATE OUT OF SUSPENSION.
 3. INSPECT AND REPAIR FILTERS AFTER EACH STORM EVENT. REMOVE SEDIMENT WHEN 1/2 OF THE FILTER DEPTH HAS BEEN FILLED. REMOVED SEDIMENT SHALL BE DEPOSITED IN AN AREA TRIBUTARY TO A SEDIMENT BASIN OR OTHER FILTERING MEASURE.
 4. SEDIMENT AND GRAVEL SHALL BE IMMEDIATELY REMOVED FROM TRAVELED WAY OF ROAD.
 5. GRAVEL BAG INLET PROTECTION ON ROADS OPEN TO THE PUBLIC WILL REQUIRE DELINEATION DEVICES TO ALERT MOTORISTS, BICYCLISTS AND PEDESTRIANS. THE USE OF SUCH DEVICES SHALL BE SUBJECT TO THE ENGINEERS APPROVAL.



NOTE: REFER TO 2005 CALTRANS STANDARD PLAN TS1 FOR TEMPORARY SILT FENCE SPECIFICATIONS AND DETAILS.

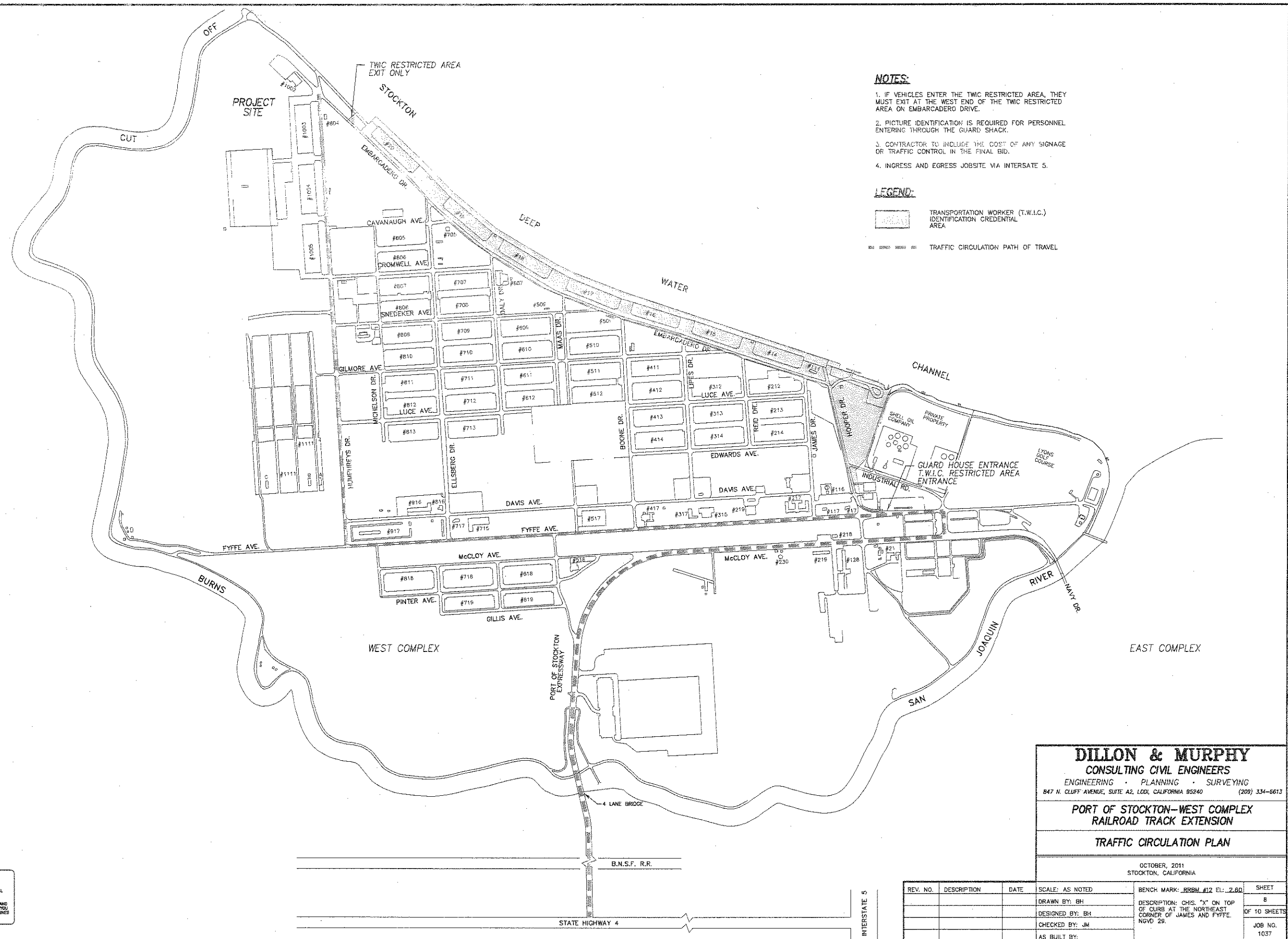
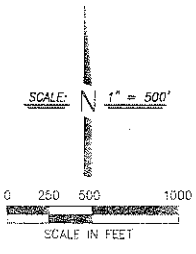
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	DESIGNED BY: MT				OF 10 SHEETS
	CHECKED BY: JM				JOB NO. 1037
	AS BUILT BY:				

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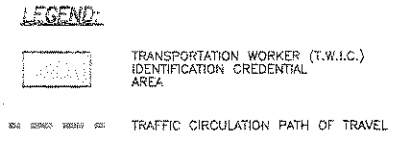
PORT OF STOCKTON-WEST COMPLEX RAILROAD TRACK EXTENSION

EROSION CONTROL PLAN

OCTOBER, 2011
STOCKTON, CALIFORNIA



- NOTES:**
1. IF VEHICLES ENTER THE TWC RESTRICTED AREA, THEY MUST EXIT AT THE WEST END OF THE TWC RESTRICTED AREA ON EMBARCADERO DRIVE.
 2. PICTURE IDENTIFICATION IS REQUIRED FOR PERSONNEL ENTERING THROUGH THE GUARD SHACK.
 3. CONTRACTOR TO INCLUDE THE COST OF ANY SIGNAGE OR TRAFFIC CONTROL IN THE FINAL BID.
 4. INGRESS AND EGRESS JOBSITE VIA INTERSTATE 5.



BEFORE YOU DIG
 CALL USA UNDERGROUND SERVICE ALERT
 48 HOURS PRIOR TO CONSTRUCTION. CALL
 TOLL FREE 1-800-227-3800
 KEEP ALL VEHICLES, EQUIPMENT, TOOLS AND
 PEOPLE 10 FEET FROM POWER LINES. IF YOU
 MUST WORK WITHIN 10 FEET OF POWER LINES
 PLEASE CALL PG&E @ 800-743-5000

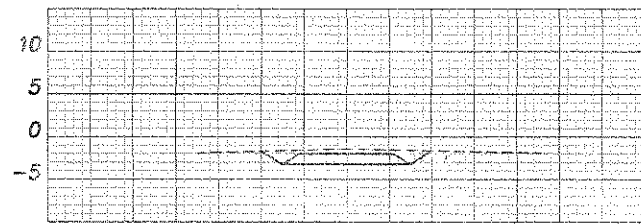
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**PORT OF STOCKTON - WEST COMPLEX
 RAILROAD TRACK EXTENSION**

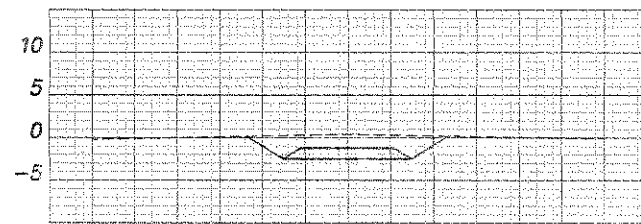
TRAFFIC CIRCULATION PLAN

OCTOBER, 2011
 STOCKTON, CALIFORNIA

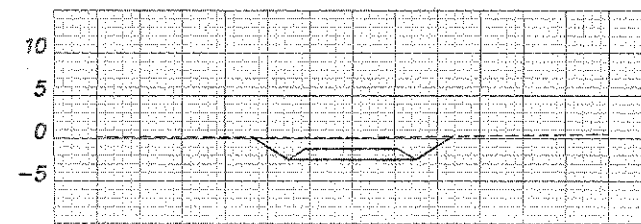
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	DESIGNED BY: BH			NGVD 29.	OF 10 SHEETS
	CHECKED BY: JM				JOB NO.
	AS BUILT BY:				1037



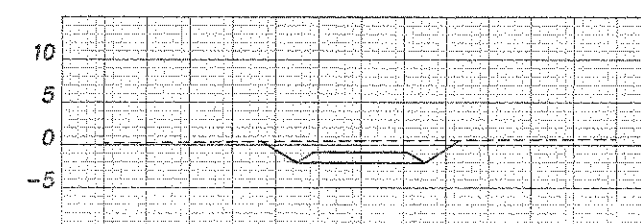
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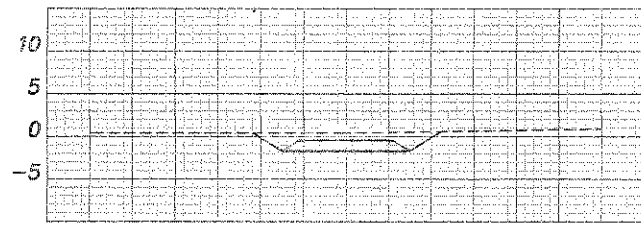
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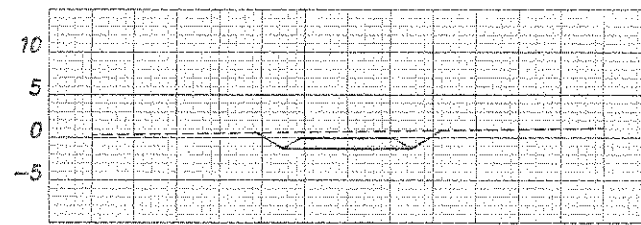
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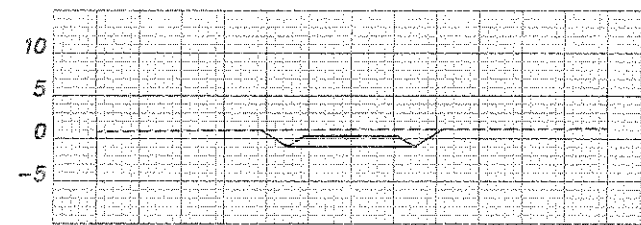
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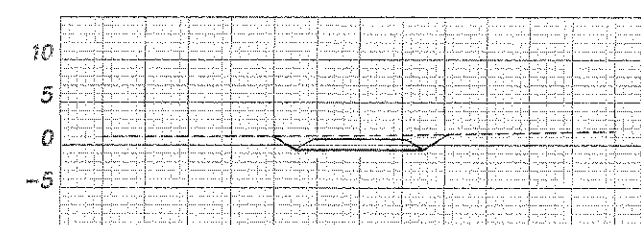
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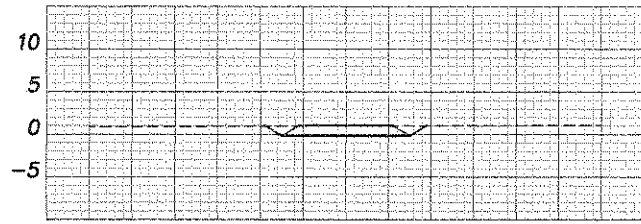
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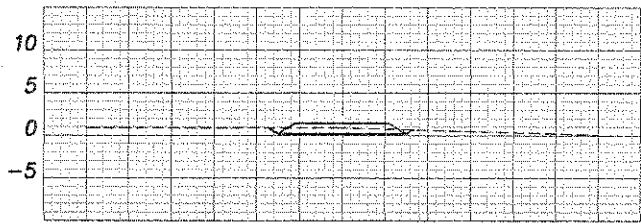
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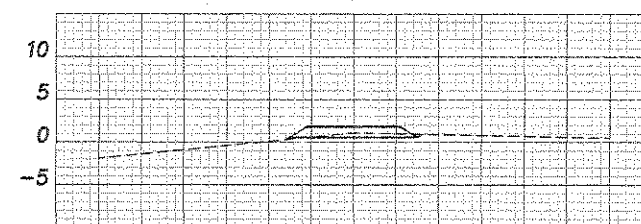
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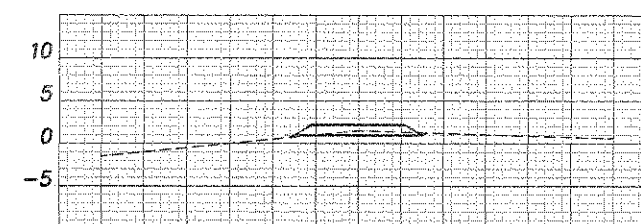
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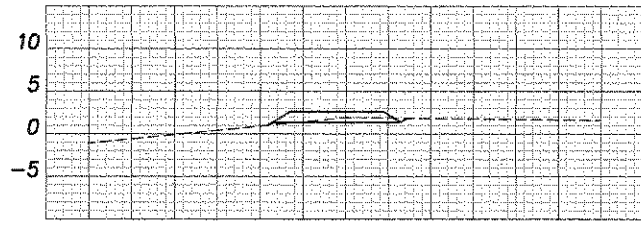
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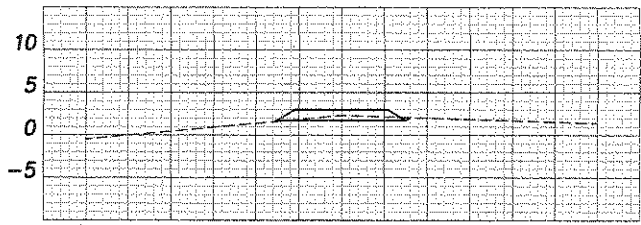
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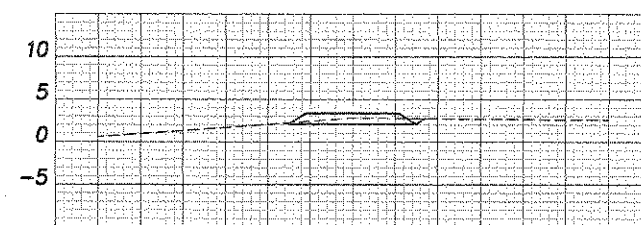
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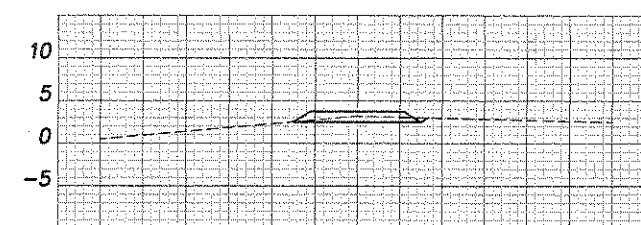
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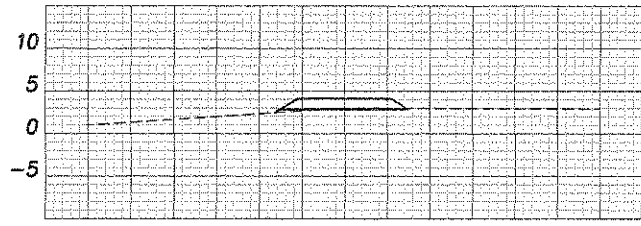
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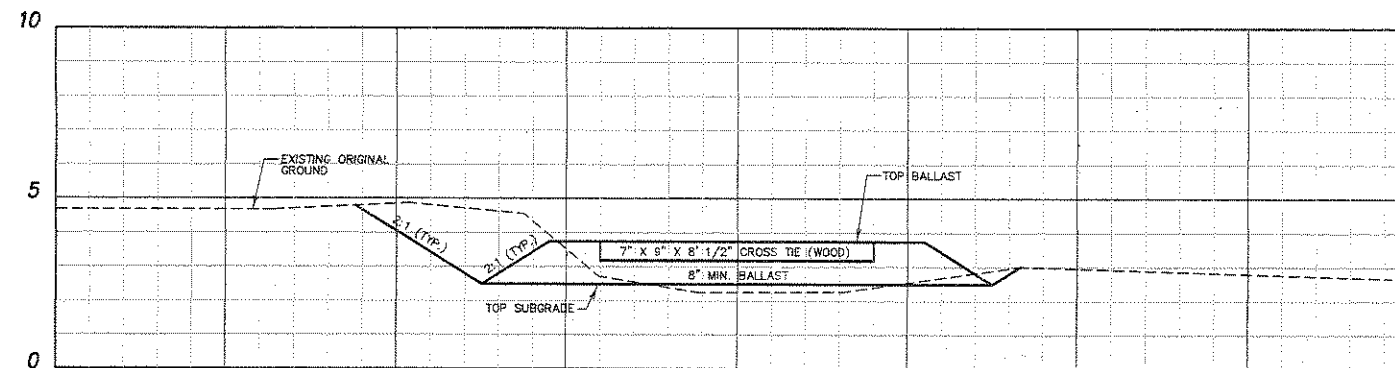
STA. 14 + 00



STA. 15 + 00



STA. 16 + 00



TYPICAL SECTION
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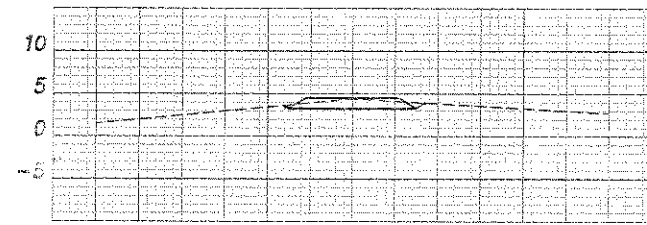
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**PORT OF STOCKTON-WEST COMPLEX
RAILROAD TRACK EXTENSION**

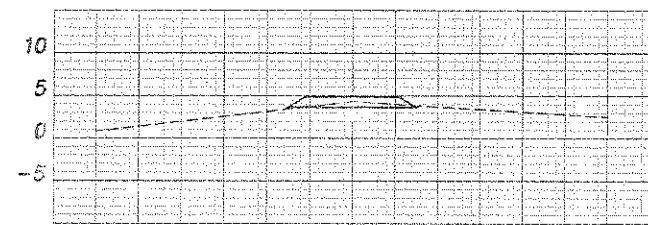
CROSS SECTIONS

OCTOBER, 2011
STOCKTON, CALIFORNIA

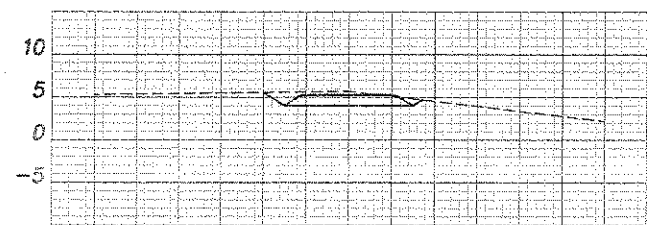
REV. NO.	DESCRIPTION	DATE	SCALE: 1" = 10'	BENCH MARK: RRB#12 EL. 2.60	SHEET
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	CHECKED BY: JM				JOB NO.
	AS BUILT BY:				1037



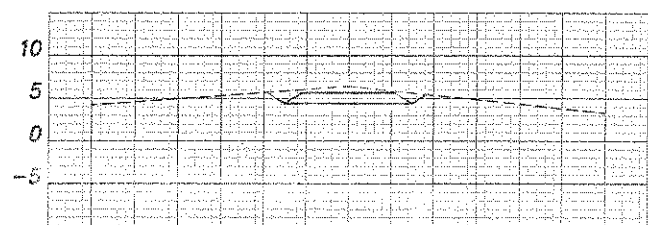
STA. 17 + 00



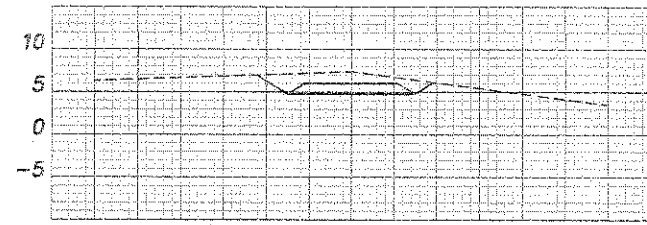
STA. 18 + 00



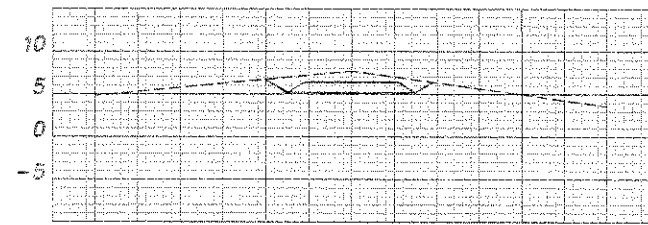
STA. 19 + 00



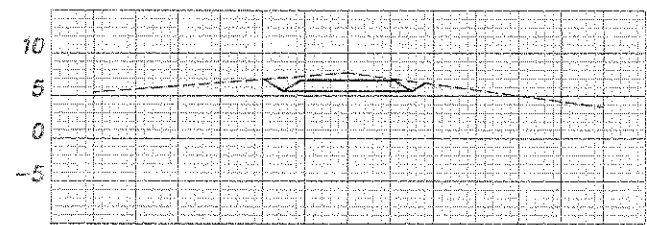
STA. 20 + 00



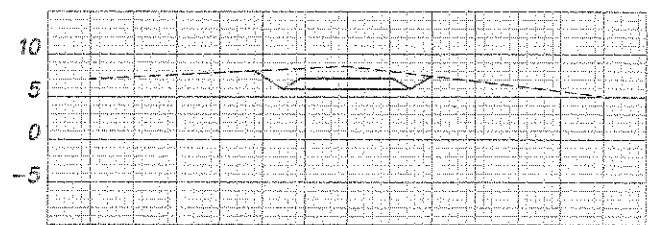
STA. 21 + 00



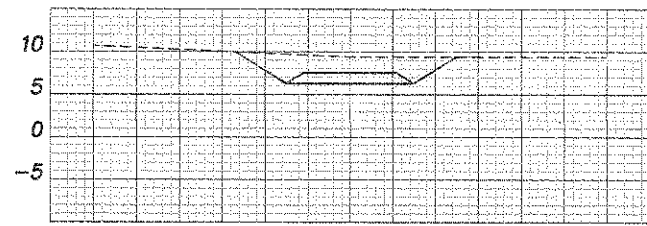
STA. 22 + 00



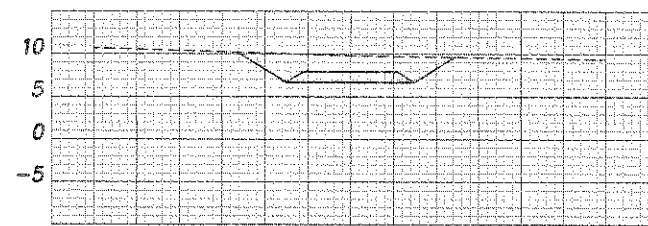
STA. 23 + 00



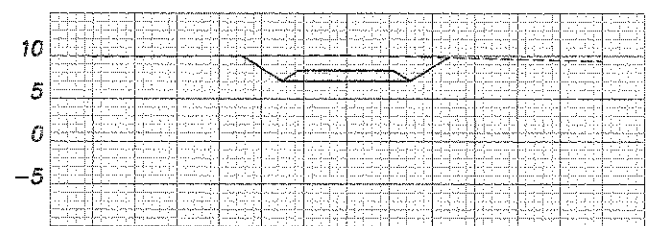
STA. 24 + 00



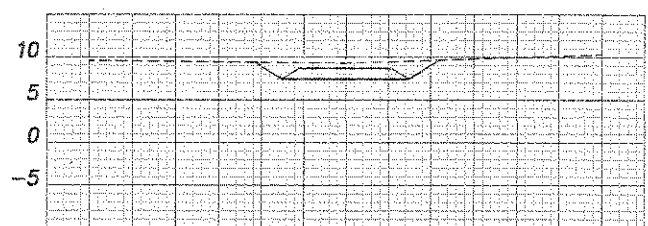
STA. 25 + 00



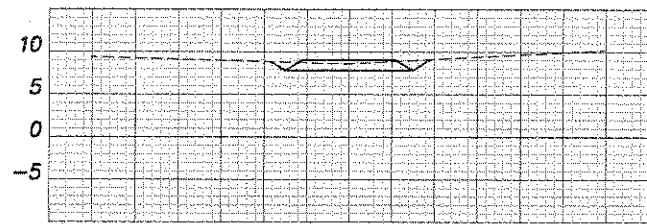
STA. 26 + 00



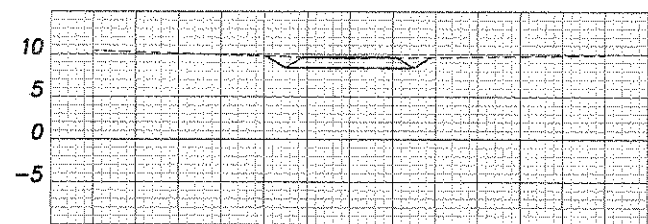
STA. 27 + 00



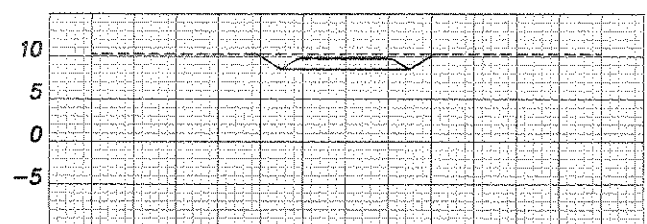
STA. 28 + 00



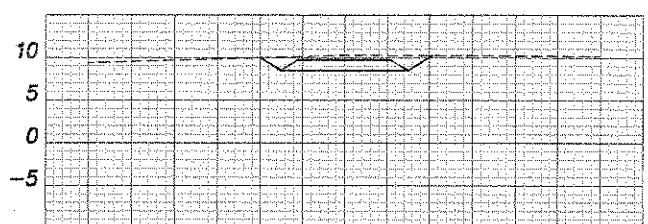
STA. 29 + 00



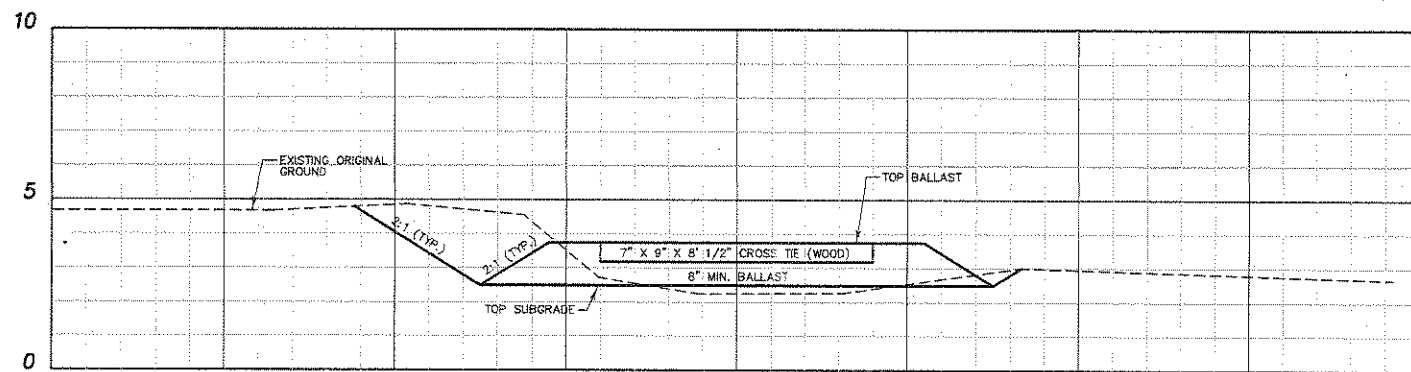
STA. 30 + 00



STA. 31 + 00



STA. 32 + 00



TYPICAL SECTION
NOT TO SCALE

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**PORT OF STOCKTON-WEST COMPLEX
RAILROAD TRACK EXTENSION**

CROSS SECTIONS

REV. NO.	DESCRIPTION	DATE	SCALE: 1" = 10'	BENCH MARK: RBBM #12 EL.: 2.60	SHEET
			DRAWN BY: MT	DESCRIPTION: CHIS. "x" ON TOP OF CURB AT THE NORTHEAST CORNER OF JAMES AND FYFFE, NGVD 29.	10
			DESIGNED BY:		OF 10 SHEETS
			CHECKED BY: JM		JOB NO.
			AS BUILT BY:		1037